

Vorrichtungen zur Überwindung großer Gefälle.

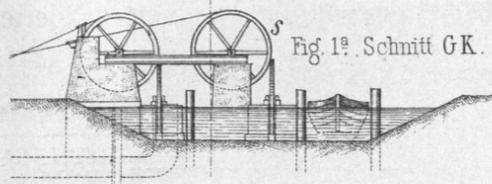
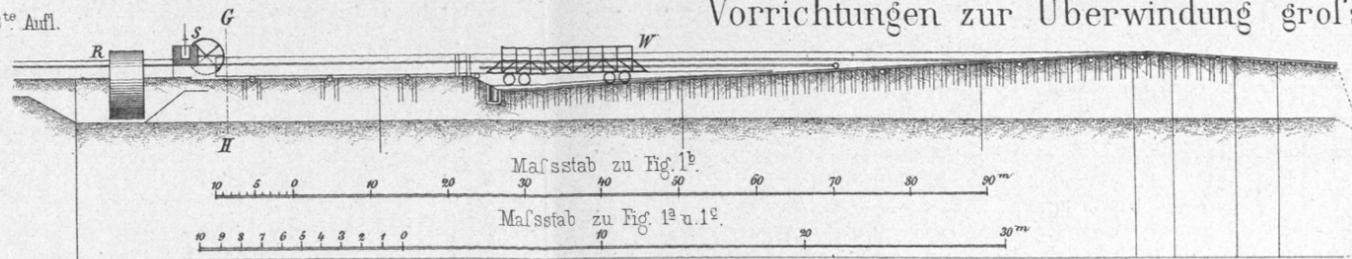
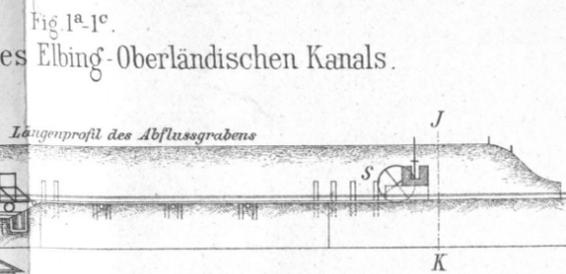


Fig. 1^a. Schnitt GK.



Geneigte Ebene des Elbing-Oberländischen Kanals.

Fig. 1^b. Längenprofil.



Längenprofil des Abflussgrabens

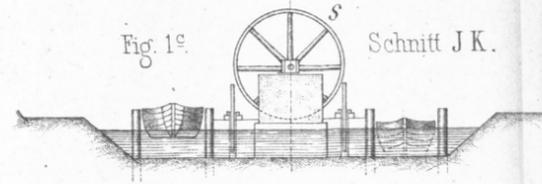


Fig. 1^e. Schnitt JK.

Fig. 2^a Ansicht vom Unterwasser aus.

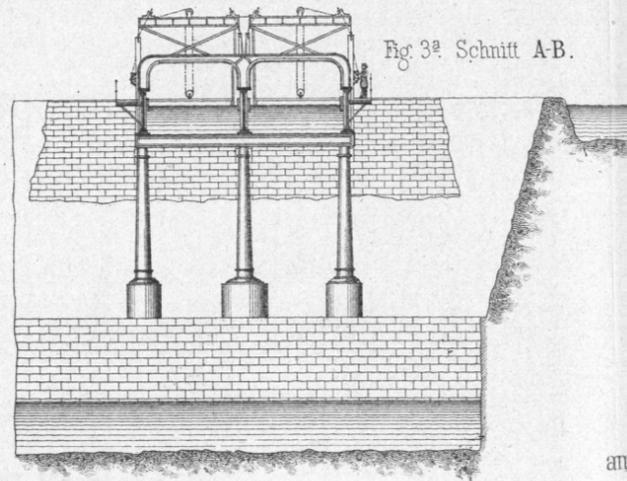
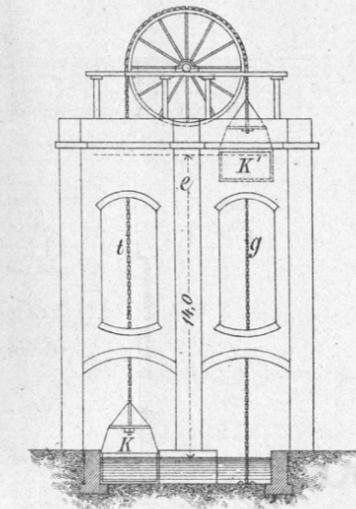


Fig. 3^a. Schnitt A-B.

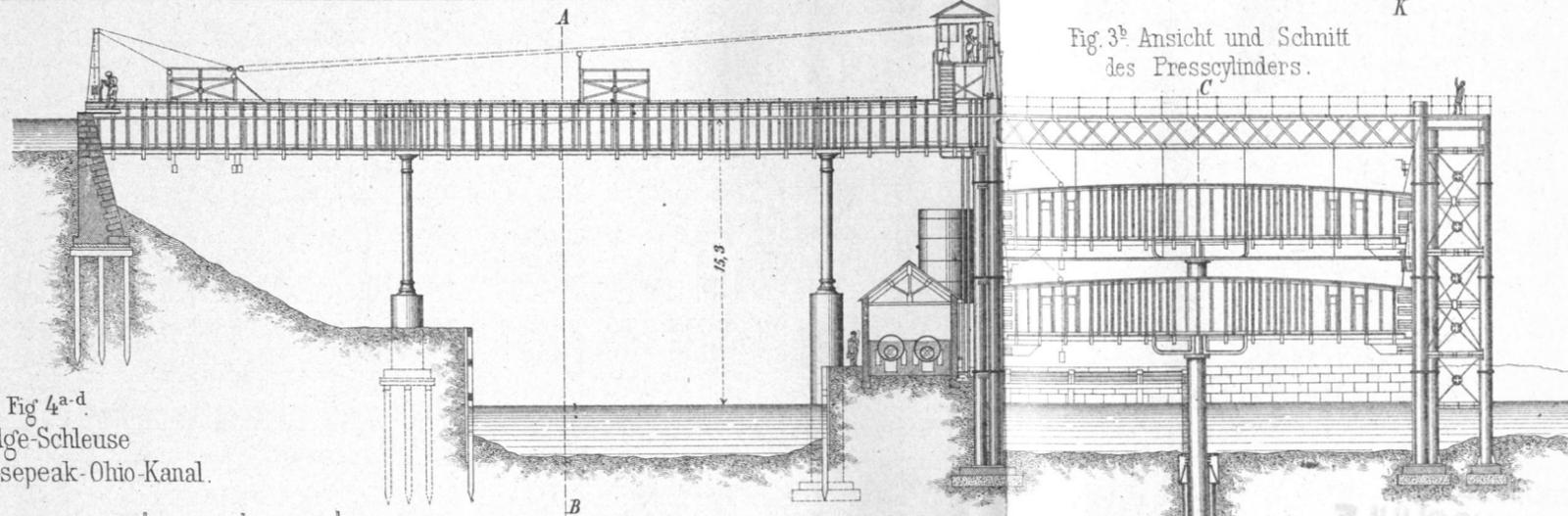


Fig. 3^b Ansicht und Schnitt des Presszylinders.

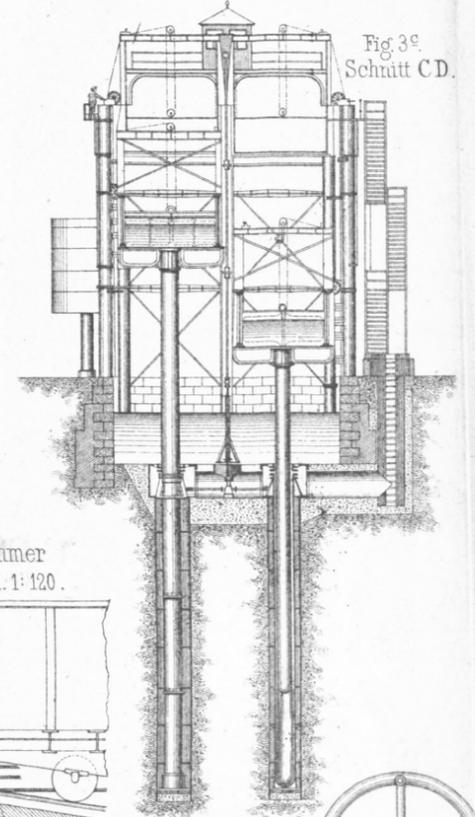


Fig. 3^c Schnitt C-D.

Fig. 4^{a-d} Dodge-Schleuse am Cheasepeak-Ohio-Kanal.

Fig. 3^d Anschluss der Schleusen-kammer an den Aquaduct.

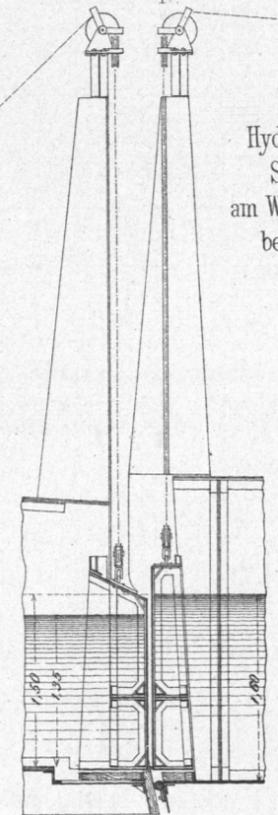


Fig. 3^e Regulierungs-Heber.

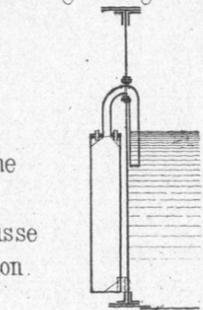


Fig. 3^{a-g} Hydraulische Schleuse am Weaverflusse bei Anderton.

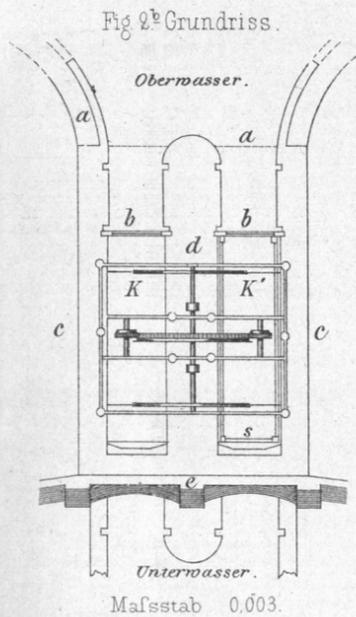


Fig. 2^b Grundriss.

Fig. 2^a u 2^b Schleuse mit beweglichen Kammern des Grand Western Kanals.

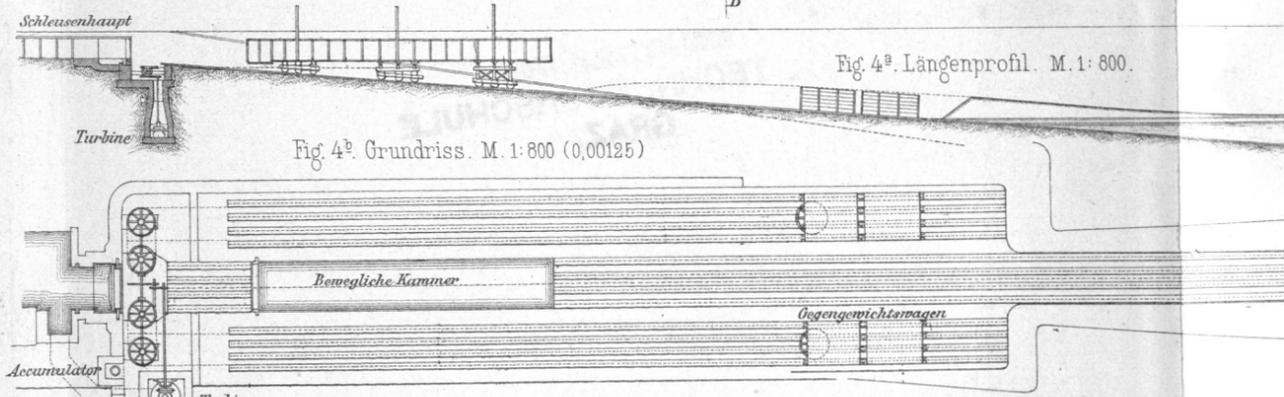


Fig. 4^b Grundriss. M. 1:800 (0,00125)

Fig. 4^a Längenprofil. M. 1:800.

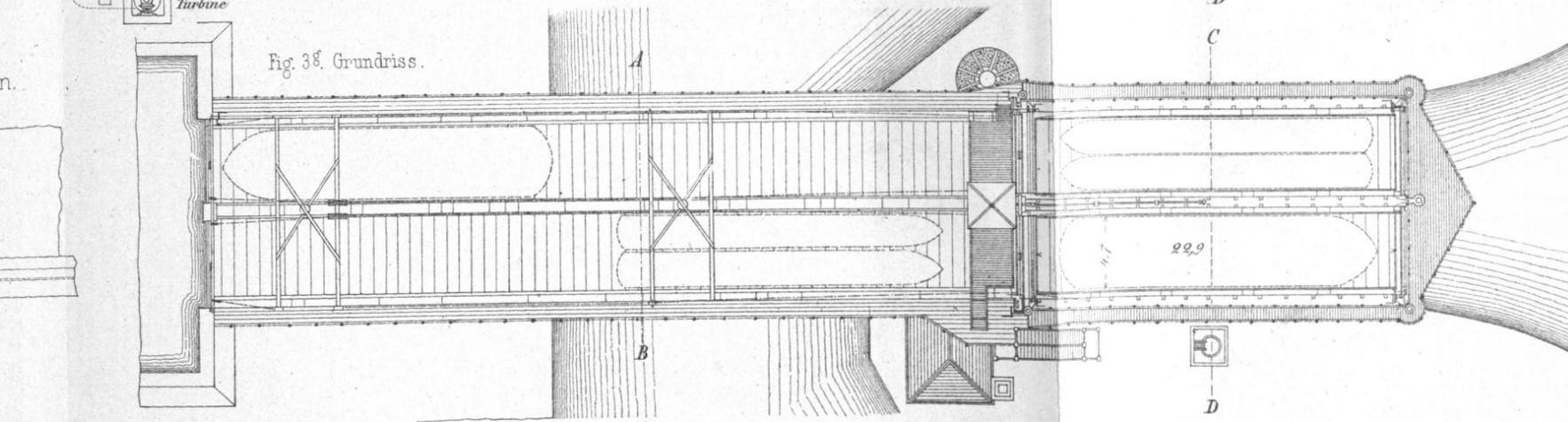


Fig. 3^f Grundriss.

Maßstab zu Fig. 3^{a, b, c, d, e, f} 0,0025.

Fig. 4^c Anschluss der Kammer an das Schleusen-haupt M. 1:120.

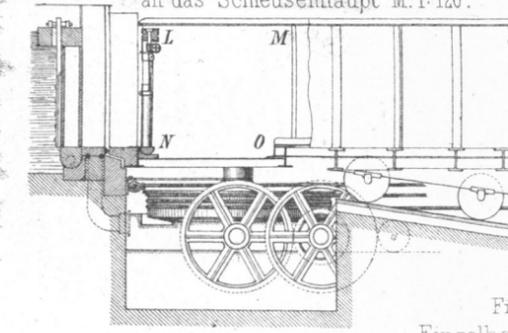


Fig. 4^d Einzelheiten. M. 1:36.

