

Vorrichtungen zur Überwindung großer Gefälle.

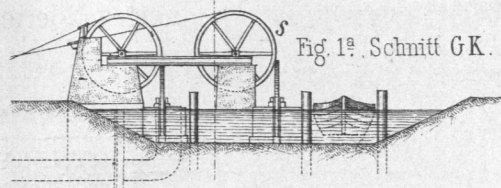
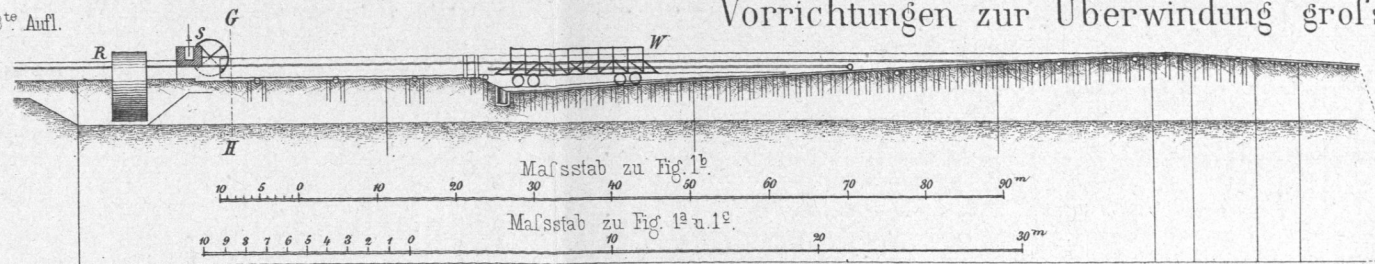
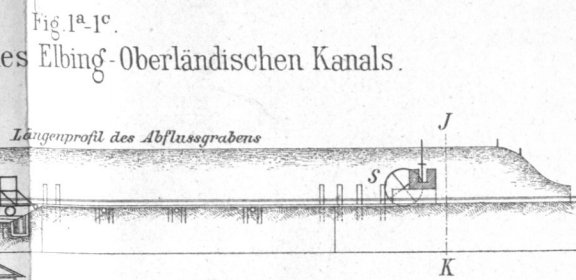


Fig. 1^a. Schnitt GK.



Geneigte Ebene des Elbing-Oberländischen Kanals.

Fig. 1^b. Längenprofil.



Längenprofil des Abflussgrabens

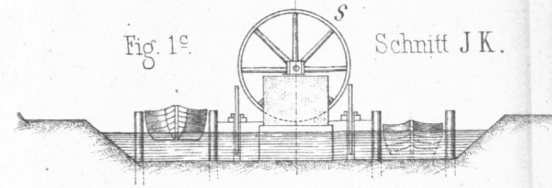


Fig. 1^e. Schnitt JK.

Fig. 2^a Ansicht vom Unterwasser aus.

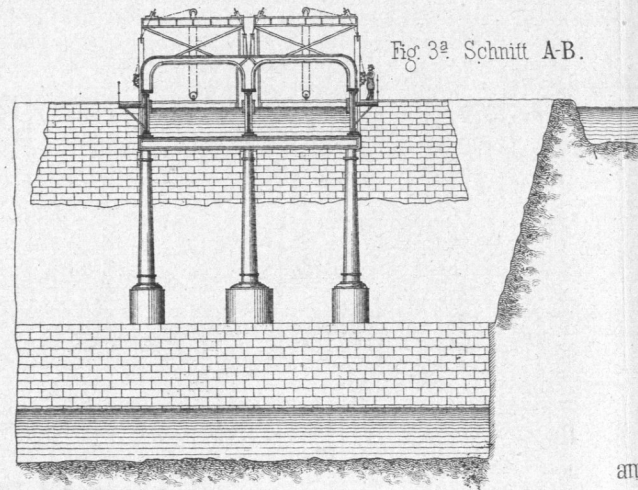
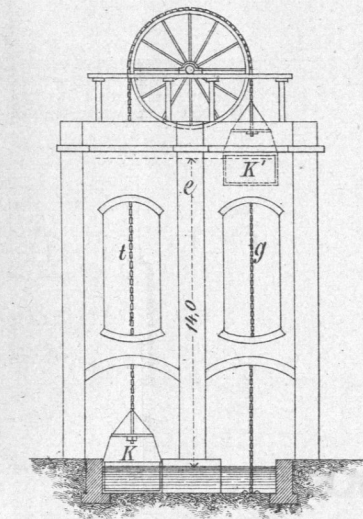


Fig. 3^a. Schnitt A-B.

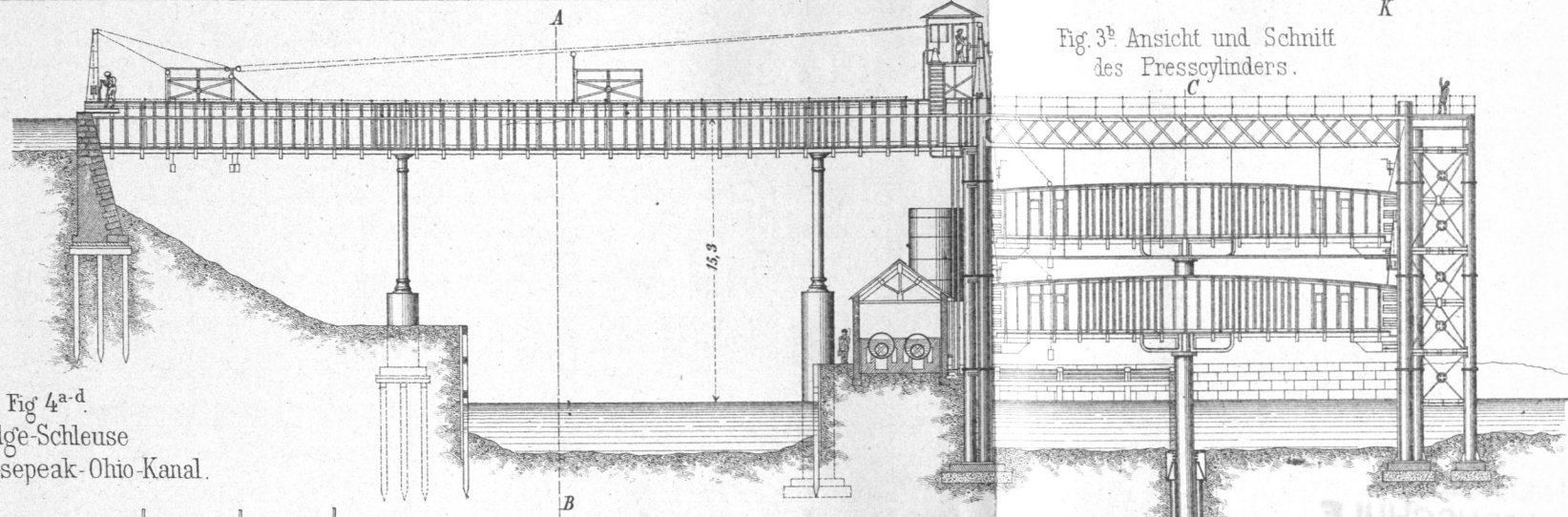


Fig. 3^b Ansicht und Schnitt des Presszylinders.

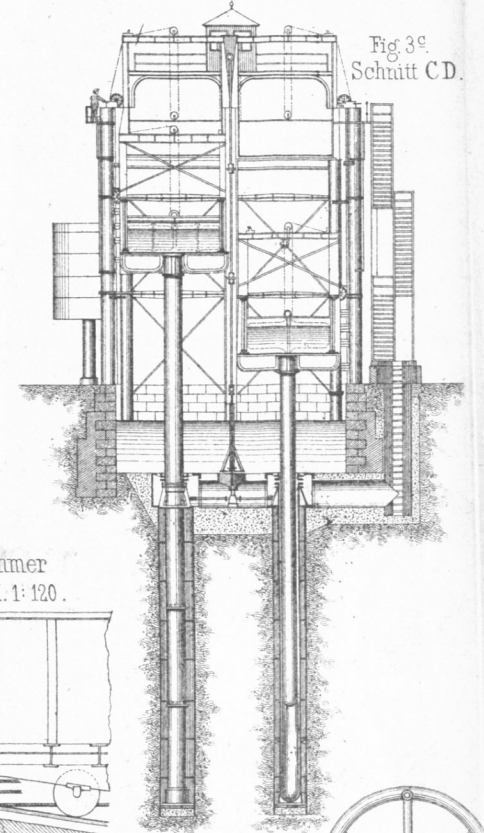


Fig. 3^c Schnitt C-D.

Fig. 4^{a-d} Dodge-Schleuse am Cheasepeak-Ohio-Kanal.

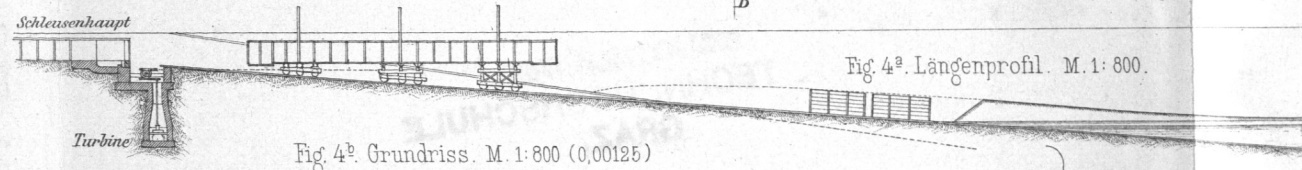


Fig. 4^a. Grundriss. M. 1:800 (0,00125)

Fig. 4^b. Längenprofil. M. 1:800.

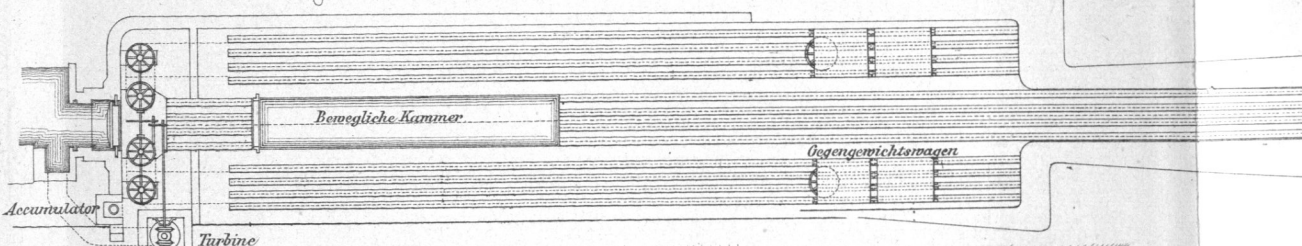


Fig. 4^c Anschluss der Kammer an das Schleusenhaupt M. 1:120.

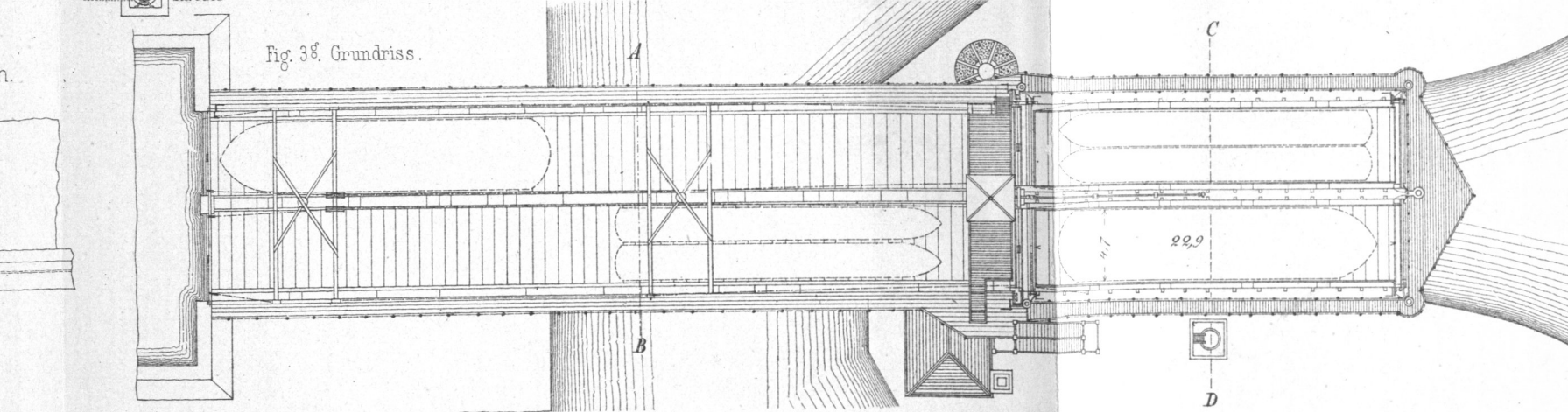


Fig. 3^e. Grundriss.

Maßstab zu Fig. 3^{a, b, c, e} 0,0025.

Fig. 3d: Anschluss der Schleusenkammer an den Aquaduct.

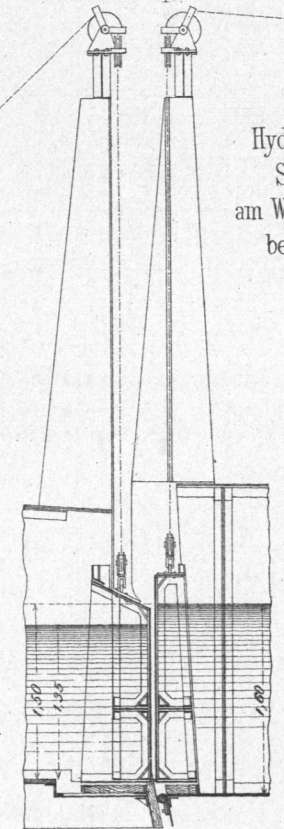


Fig. 3f: Regulierungs-Heber.

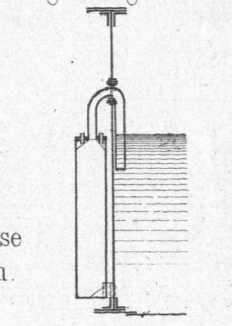


Fig. 3a-g: Hydraulische Schleuse am Weaverflusse bei Anderton.

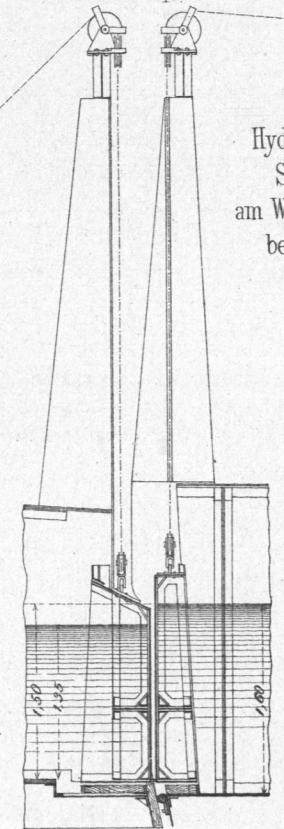
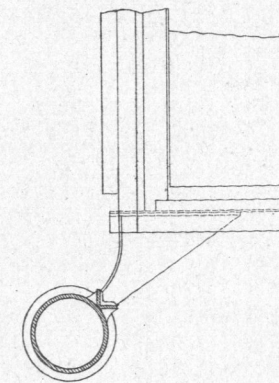


Fig. 3f: Führung der Schleusenkammern.



Maßstab zu Fig. 3d-3f.

Fig. 2b: Grundriss.

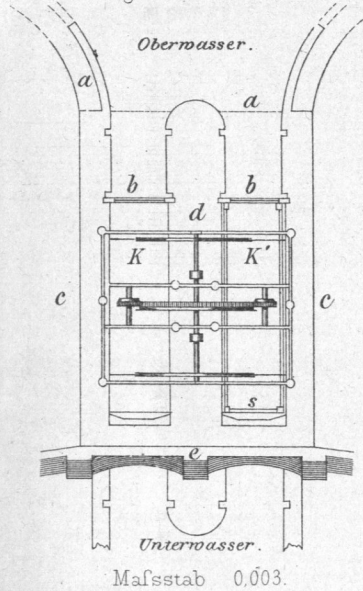


Fig. 2a u 2b: Schleuse mit beweglichen Kammern des Grand Western Kanals.

Fig. 4d: Einzelheiten. M. 1:36.

